

BOARD OF COUNTY COMMISSIONERS
SARPY COUNTY, NEBRASKA

RESOLUTION AUTHORIZING CHAIR TO SIGN SUPPLEMENTAL AGREEMENT
FOR USE OF UNION PACIFIC RIGHT-OF-WAY NEAR CAPEHART ROAD AND
FORT CROOK ROAD

WHEREAS, pursuant to Neb. Rev. Stat. §23-104(6) (Reissue 2007), the County has the power to do all acts in relation to the concerns of the county necessary to the exercise of its corporate powers; and,

WHEREAS, pursuant to Neb. Rev. Stat. §23-103 (Reissue 2007), the powers of the County as a body are exercised by the County Board; and,

WHEREAS, pursuant to Neb. Rev. Stat. 39-1402(Reissue 2008) the County Board has the power and authority of improvement, and maintenance of public roads of the County; and,

WHEREAS, Sarpy County is the successor party to an agreement with the Union Pacific Railroad Company for license to cross the railroad right of way near Fort Crook Road; and,

WHEREAS, a supplemental agreement has been proposed to accommodate the extension of the Offutt Bicycle Route Connector along Capehart Road.

NOW, THEREFORE, BE IT RESOLVED BY THIS BOARD OF COUNTY COMMISSIONERS THAT THE COUNTY OF SARPY, STATE OF NEBRASKA, that the Supplemental Agreement with the Union Pacific Railroad, as shown on the attached Exhibit "A", is hereby authorized and approved, and the Chairman is hereby authorized to sign agreement and such other documents as may be necessary to consummate the terms as described therein.

Dated this 1st day of February, 2011.

Moved by Jim Warren, seconded by Jim Thompson, that the above Resolution be adopted. Carried.

YEAS:

NAYS:

ABSENT:

Reseyah

none

Tom Richards

Amulkin

J. Thompson
J. H. H. H.

ABSTAIN:

none

Debra J. Houghtaling
County Clerk



Approved as to form:

[Signature]
Deputy County Attorney



February 11, 2011

UPRR Folder No. 2640-36

**MR CHRIS VANCE
SARPY COOUNTY
1210 GOLDEN GATE DR
PAPILLION NE 68046-2895**

Dear Mr. Vance:

Attached is your original copy of a Supplemental Agreement, fully executed on behalf of the Railroad Company.

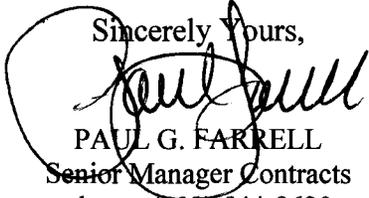
In order to protect the Railroad Company's property as well as for safety reasons, it is imperative that you notify the Railroad Company's Manager of Track Maintenance and the Communications Department:

*Mike Blackley
Manager Public Projects
Union Pacific Railroad Company
1400 Douglas Street, MS 0910
Omaha, NE 68179-0910
Phone: 402-544-2029
Fax: 402-501-0430*

*Fiber Optics Hot Line
1-800-336-9193*

If you have any questions, please contact me.

Sincerely Yours,


PAUL G. FARRELL
Senior Manager Contracts
phone: (402) 544-8620
e-mail: pgfarrell@up.com

Real Estate Department
UNION PACIFIC RAILROAD COMPANY
1400 Douglas Street, MS 1690
Omaha, Nebraska 68179-1690
fax: 402.501.0340



UPRR Folder No. 2640-36
UPRR Audit No. CA52831

SUPPLEMENTAL AGREEMENT

This Supplemental Agreement ("Supplement") is made as of the 11th day of February, 2011 ("Effective Date"), by and between **UNION PACIFIC RAILROAD COMPANY**, a Delaware corporation ("Railroad") and the **COUNTY OF SARPY** ("County").

RECITALS:

- A. By instrument dated July 22, 1960, the Railroad and the State of Nebraska, by and through its Department of Roads, entered into an agreement identified in Railroad's records as its Audit No. CA52831, covering the construction, use, maintenance and repair of a grade separated overpass structure (the "Structure") to carry vehicular traffic on Capehart Road over and across Railroad's property and trackage at Railroad Mile Post 470.66 on Railroad's Fall City Subdivision in Sarpy County, Nebraska.
- B. The County has assumed ownership and maintenance responsibilities of the Structure from the State.
- C. The general location of the Structure is shown and marked on the Railroad Location Print marked **Exhibit A**, attached hereto and hereby made a part hereof.
- D. As part of the Offutt Bicycle Route Connector along Capehart Road near the Offutt Air Force Base gate to the Papillion Creek's Keystone Trail (the "Trail Connector"), the County as its project (the "Project") desires to (i) construct a portion of the Trail Connector on and across the Structure, (ii) remove existing fencing on the Structure, and (iii) construct new fencing on the Structure (the "Fencing").
- E. The portion of the Trail Connector referenced in Paragraph C above that is to be constructed on the Structure and/or crosses over the Railroad's property and trackage is hereinafter referred to as the "Trail."
- F. The Railroad and the County are entering into this Supplement to cover the above.

AGREEMENT:

NOW, THEREFORE, IT IS MUTUALLY AGREED BY AND BETWEEN THE COUNTY AND THE RAILROAD AS FOLLOWS:

Section 1. RIGHT GRANTED.

- A. In consideration of the terms and conditions of this Agreement, the Railroad hereby grants to the County, its successors and assigns, the right to construct, use, maintain and repair the Trail and Fencing.
- B. The right granted herein is made subject to and subordinate to the prior and continuing right and obligation of Railroad, its successors and assigns, to use all of its tracks and property

underlying the Trail and Fencing in the performance of its duty as a common carrier, and there is reserved unto Railroad, and its successors and assigns, the right (consistent with the rights granted herein to County) to construct, reconstruct, maintain, repair, use and operate existing or future additional railroad tracks, track appurtenances, fiber optic or signal lines and facilities, pipe, and wire lines on its property.

C. The right granted herein is also made subject to all outstanding leases, licenses and other outstanding rights, including but not limited, those for pipe, telephone, electric and fiber optic lines and the right of renewals and the extensions of the same, and subject also to all conditions, limitations, restrictions, encumbrances, reservations or interests of any person or entity which may affect the Trail and Fencing, whether recorded or unrecorded.

D. The right granted herein is also limited to such rights as the Railroad may have in its right of way and is granted without warranty, express or implied.

Section 2. CONSTRUCTION AND MAINTENANCE OF TRAIL AND FENCING.

The County, at its expense, shall construct, maintain and repair the Trail and Fencing in compliance with the Plans (as defined in Section 4) and all applicable federal, County and local laws, rules and regulations.

Section 3. NO TRESPASSING ONTO RAILROAD PROPERTY.

The County, at its expense, shall take all reasonable measures to prevent pedestrians using the Trail Connector from trespassing on to Railroad's property, including, without limitation, the installation of fencing on both sides on the portions of the Trail Connector located adjacent to Railroad's right of way if Railroad should deem such fencing necessary to prevent pedestrians from trespassing on Railroad's right of way.

Section 4. PLANS.

A. The Railroad has approved the Trail and Fencing Plans marked **Exhibit B**, attached hereto and hereby made a part hereof (the "Plans").

B. No changes in the Plans affecting the interests of the Railroad or the County shall be made unless the Railroad and the County have consented to such changes in writing.

C. The Railroad's review and approval of the Plans will in no way relieve the County or its Contractor from their responsibilities, obligations and/or liabilities under this Agreement, and will be given with the understanding that the Railroad makes no representations or warranty as to the validity, accuracy, legal compliance or completeness of the Plans and that any reliance by the County or Contractor on the Plans is at the risk of the County and Contractor.

Section 5. INDEMNITY.

The County, only to the extent permitted by applicable Nebraska statutes, agrees to hold harmless the Railroad, its officers, agents and employees, from and against any and all liability, loss, damage, claims, demands, actions, causes of action, costs and expense (collectively, "Losses") which may result from personal injury to or death of persons whomsoever or damage to or loss or destruction of property whatsoever, when such Losses grow out of or arise from the existence of the Trail and/or any trespass of Railroad's property by a user of the Trail.

Section 6. CONTRACTOR'S INDEMNITY AND INSURANCE.

A. The County agrees to insert the following provision in any contract in which County's Contractor will be working on the Trail and/or Fencing work:

"To the extent allowed by law, Contractor hereby releases and shall indemnify, defend and hold harmless Railroad, its affiliated companies, partners, successors, assigns, legal representatives, officers, directors, shareholders, employees and agents for, from and against any and all claims, liabilities, fines, penalties, damages, losses, liens, causes of action, actions, suits, demands, costs and expenses (including, without limitation, court costs and reasonable attorneys' fees) and judgments (collectively, "Claims"), of any nature, kind or description of any person (including, without limitation, the employees of the parties hereto and the employees of Railroad) or entity directly or indirectly arising out of, resulting from, or related to (in whole or in part) (i) the use, occupancy or presence of County, Contractor, or its subcontractors, employees or agents, in, on, or about Railroad's property pursuant to this Agreement; (ii) the performance or failure to perform by Contractor, its subcontractors, employees, or agents, its work or any obligation under this Agreement; or (iii) the sole or contributing acts or omissions of Contractor, its subcontractors, employees, or agents, in, on, or about Railroad's property pursuant to this Agreement. Contractor's duty to defend under the above indemnity provision includes, without limitation, the obligation of Contractor to appear and defend in the name of Railroad, any suits or actions brought against Railroad with respect to any such Claims, and to pay and satisfy any final judgment that may be rendered against Railroad in any such suit or action. The liability and duty to defend assumed by Contractor under the above indemnity provision will not be affected by the fact, if it is a fact, that the claim was occasioned by or contributed to by the negligence of Railroad, its agents, servants, employees, or otherwise, except (i) to the extent that such claim was proximately caused by the active gross negligence or intentional misconduct of Railroad or its employees, servants or agents, or (ii) for claims arising solely out of the negligent actions of Railroad or its employees, servants or agents."

B. The County also agrees to require its Contractor, prior to performing any Trail or Fencing work, to provide to the Railroad representative named in Section 7, the insurance coverage, certificates, endorsements and binders that are described in the Insurance Requirements marked **Exhibit C**, attached hereto and hereby made a part hereof.

Section 7. RAILROAD REPRESENTATIVE.

The Railroad representative for this Project is:

*Mike Blackley
Manager-Industry & Public Projects
Union Pacific Railroad Company
Engineering Department - MS 0910
1400 Douglas Street
Omaha, NE 68179
Phone: 402.544.2029*

Section 8. SUPPLEMENT IS SUPPLEMENTAL TO ORIGINAL AGREEMENT.

A. This Supplement is supplemental to the Original Agreement and nothing herein shall be construed as amending or modifying the Original Agreement except as herein specifically provided.

B. If there is a conflict of terms and conditions between this Supplement and the Original Agreement, the terms and conditions of this Supplement shall govern.

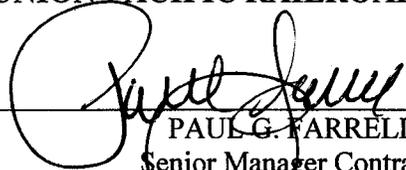
Section 9. EFFECTIVE DATE.

This Supplement is effective as of the Effective Date first herein written.

IN WITNESS WHEREOF, the parties have duly executed this Supplement as of the Effective Date first herein written.

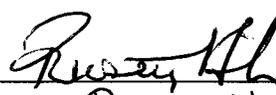
UNION PACIFIC RAILROAD COMPANY

By


PAUL G. FARRELL
Senior Manager Contracts

COUNTY OF SARPY

By

 vice Chairman, Sarpy Co. Board

Printed Name: Rusty Hike

Title: Vice Chairman, Sarpy Co. Board

Approved as to form:


County Attorney



SARPY COUNTY

Dennis L. Wilson P.E.
Sarpy County Engineer

SURVEY / HIGHWAY DEPARTMENT
15100 South 84th Street • Papillion, Ne 68046-2895
Phone (402) 537-6900 • FAX (402) 537-6955 • www.sarpy.com

Memo

To: Sarpy County Board of Commissioners
From: Dennis Wilson, Sarpy County Engineer
Date: 1/28/2011
Re: *Approval of the Supplemental Agreement with Union Pacific / Pedestrian Crossing near Capehart Road and Fort Cook.*

I recommend approval of the above mentioned Supplemental Agreement for the use of Union Pacific right-of-way near Capehart Road and Fort Cook Road. This agreement has been proposed to accommodate the extension of the Offutt Bicycle Route connector along Capehart Road.

If you have any questions, please feel free to contact me.



OFFUTT BICYCLE ROUTE CONNECTOR

PROJECT NO.
137624

BELLEVUE, NEBRASKA
SEPTEMBER 17, 2010



LOCATION MAP

SHEET INDEX

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- 2S SUMMARY OF QUANTITIES
- 2T1 - 2T2 TYPICAL SECTIONS
- 2H ALIGNMENT & CONTROL
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- XI - X8 CROSS SECTIONS

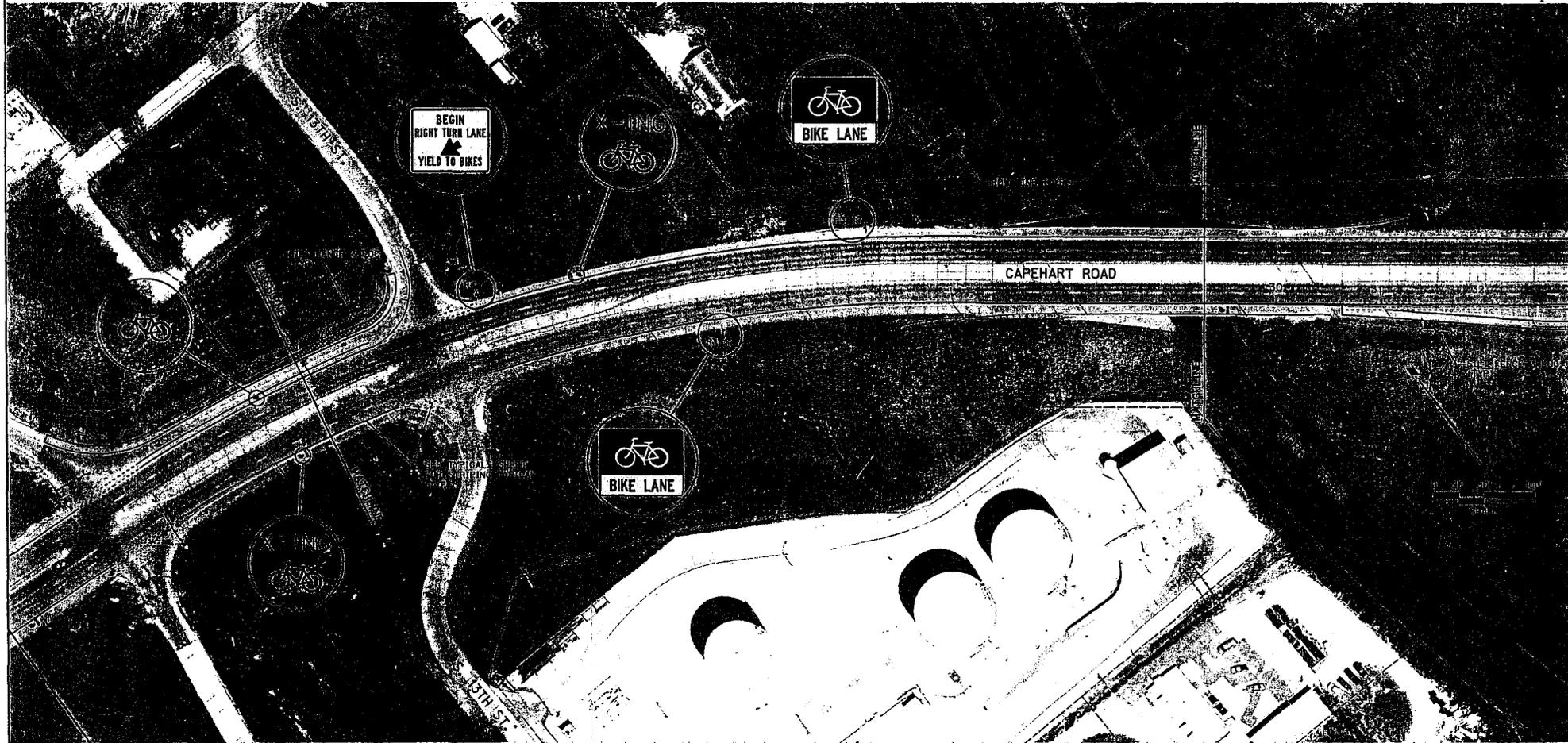
NDOR STANDARD PLANS

- PLAN 301-R10 PAVEMENT DETAILS
- PLAN 502 SILT FENCE DETAILS
- PLAN 920-R5 TRAFFIC CONTROL, CONSTRUCTION AND MAINTENANCE
- PLAN 924 TYPICAL URBAN TRAFFIC CONTROL PLAN

ROADWAY	2006 ADT
CAPEHART RD. - HWY 75 to FORT CROOK RD.	15,500
CAPEHART RD. - EAST of FORT CROOK RD.	13,500



REMOVE FENCE		
STATION TO STATION	SIDE	LIN. FT.
RAILROAD BRIDGE RAILING FENCE	NORTH	69.5
RAILROAD BRIDGE RAILING FENCE	SOUTH	69.5



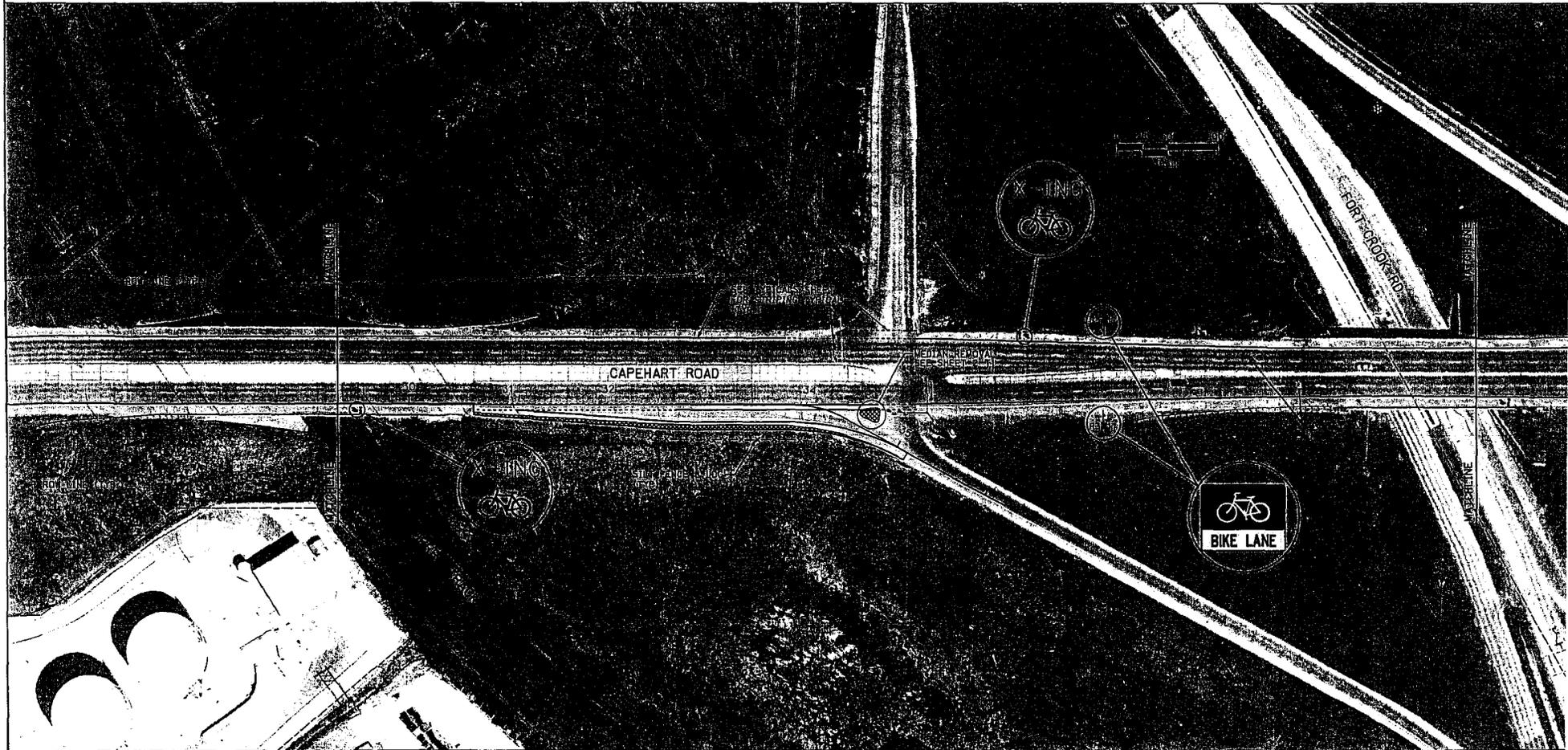
BUILD CHAIN LINK FENCE, SPECIAL PLAN 4C		
LOCATION	LIN. FT.	HEIGHT
LT & RT BRIDGE RAILING OVER UPRR TRACKS	320	5'

BUILD 9" CONCRETE PAVEMENT				
STATION	TO STATION	SIDE	SO. YDS.	
23+62	25+32	LT	66	

- x - BUILD FABRIC SILT FENCE-LOW POROSITY, PLAN 502				
STATION	TO STATION	SIDE	DESCRIPTION	LIN. FT.
23+62	25+32	LT	-	124



CONSTRUCTION



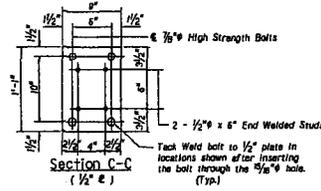
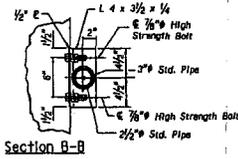
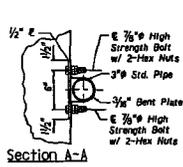
BUILD CHAIN LINK FENCE, SPECIAL PLAN 4C			
LOCATION	LIN. FT.	HEIGHT	
LT & RT BRIDGE RAILING OVER FORT CROOK ROAD	290	5'	

BUILD 9" CONCRETE PAVEMENT				
STATION TO	STATION	SIDE	SO. YDS.	
30+66	35+12	RT	165	
MEDIAN REMOVAL AREA		-	24	

BUILD FABRIC SILT FENCE-LOW POROSITY, PLAN 502				
STATION TO	STATION	SIDE	DESCRIPTION	LIN. FT.
30+66	35+12	RT	-	432



CONSTRUCTION



NOTES

All 3" and 2 1/2" standard steel pipe shall conform to the requirements of A.S.T.M. A-53, Grade B.

All 1 1/4" standard steel pipe shall conform to the requirements of A.S.T.M. A-53, Grade F.

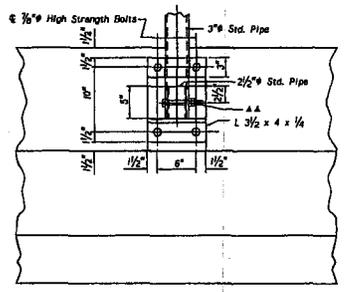
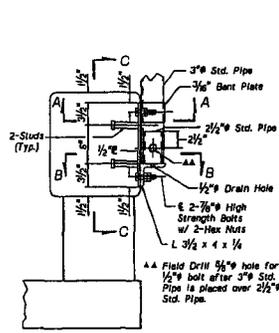
Material for railing assembly, after fabrication, shall be galvanized according to the requirements of A.S.T.M. A-153. Anchor bolts and hardware shall be galvanized according to the requirements of A.S.T.M. A-153.

Chain-Link Fence fabric shall conform to the requirements of A.S.T.M. A-362. The fabric shall be 7'-0" in height, No. 9 gauge wire, with 2" mesh, zinc coated after weaving with class 2 coating, and shall have knuckled salvage top and bottom.

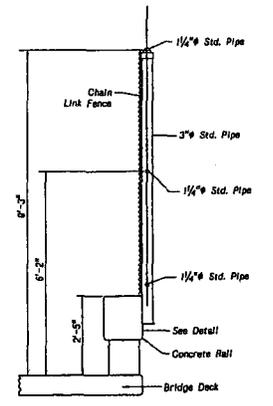
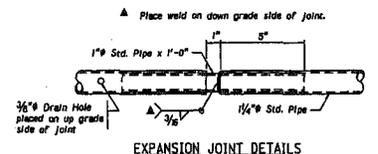
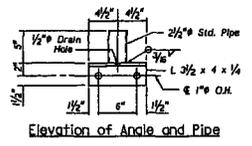
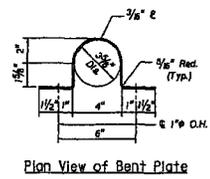
Railing shall conform to vertical alignment of the Bridge, posts shall be set plumb. Fabric shall be installed parallel to bridge floor.

All nuts shall be placed on the outside of the fence. Pen all 3/8" Bolts.

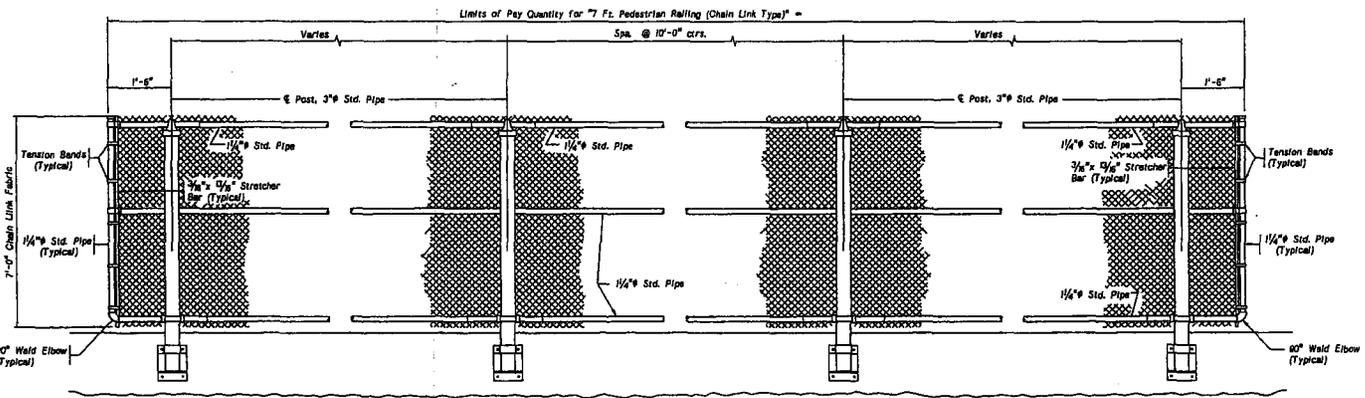
3" Std. Pipe = 3 1/2" O.D.
2 1/2" Std. Pipe = 2 7/8" O.D.
1 1/4" Std. Pipe = 1 1/2" O.D.



DETAILS FOR FENCE ATTACHMENT TO CONCRETE RAIL
Not to Scale



Fence Details at Rail



NOTE: For Chain Link Fence location see sheet of



EXHIBIT C

TO SUPPLEMENTAL AGREEMENT

INSURANCE REQUIREMENTS

Contractor shall, at its sole cost and expense, procure and maintain during the course of the Project and until all Project work on Railroad's property has been completed and the Contractor has removed all equipment and materials from Railroad's property and has cleaned and restored Railroad's property to Railroad's satisfaction, the following insurance coverage:

- A. Commercial General Liability Insurance.** Commercial general liability (CGL) with a limit of not less than \$5,000,000 each occurrence and an aggregate limit of not less than \$10,000,000. CGL insurance must be written on ISO occurrence form CG 00 01 12 04 (or a substitute form providing equivalent coverage).

The policy must also contain the following endorsement, which must be stated on the certificate of insurance:

- Contractual Liability Railroads ISO form CG 24 17 10 01 (or a substitute form providing equivalent coverage) showing "Union Pacific Railroad Company Property" as the Designated Job Site.
- Designated Construction Project(s) General Aggregate Limit ISO Form CG 25 03 03 97 (or a substitute form providing equivalent coverage) showing the project on the form schedule.

- B. Business Automobile Coverage Insurance.** Business auto coverage written on ISO form CA 00 01 10 01 (or a substitute form providing equivalent liability coverage) with a combined single limit of not less \$5,000,000 for each accident and coverage must include liability arising out of any auto (including owned, hired and non-owned autos).

The policy must contain the following endorsements, which must be stated on the certificate of insurance:

- Coverage For Certain Operations In Connection With Railroads ISO form CA 20 70 10 01 (or a substitute form providing equivalent coverage) showing "Union Pacific Property" as the Designated Job Site.
- Motor Carrier Act Endorsement - Hazardous materials clean up (MCS-90) if required by law.

- C. Workers' Compensation And Employers' Liability Insurance.** Coverage must include but not be limited to:
- Contractor's statutory liability under the workers' compensation laws of the County of Nebraska.
 - Employers' Liability (Part B) with limits of at least \$500,000 each accident, \$500,000 disease policy limit \$500,000 each employee.

If Contractor is self-insured, evidence of County approval and excess workers compensation coverage must be provided. Coverage must include liability arising out of the U. S. Longshoremen's and Harbor Workers' Act, the Jones Act, and the Outer Continental Shelf Land Act, if applicable.

The policy must contain the following endorsement, which must be stated on the certificate of insurance:

- Alternate Employer endorsement ISO form WC 00 03 01 A (or a substitute form providing equivalent coverage) showing Railroad in the schedule as the alternate employer (or a substitute form providing equivalent coverage).

- D. Railroad Protective Liability Insurance.** Contractor must maintain "Railroad Protective Liability" (RPL) insurance written on ISO occurrence form CG 00 35 12 04 (or a substitute form providing equivalent coverage) on behalf of Railroad as named insured, with a limit of not less than \$2,000,000 per occurrence and an aggregate of \$6,000,000. The definition of "JOB LOCATION" and "WORK" on the declaration page of the policy shall refer to this Agreement and shall describe all WORK or OPERATIONS performed under this agreement. Contractor shall provide this Agreement to Contractor's insurance agent(s) and/or broker(s) and Contractor shall instruct such agent(s) and/or broker(s) to procure the insurance coverage required by this Agreement. A BINDER STATING

THE POLICY IS IN PLACE MUST BE SUBMITTED TO RAILROAD BEFORE THE WORK MAY COMMENCE AND UNTIL THE ORIGINAL POLICY IS FORWARDED TO UNION PACIFIC RAILROAD.

- E. Umbrella Or Excess Insurance.** If Contractor utilizes umbrella or excess policies, these policies must "follow form" and afford no less coverage than the primary policy.
- F. Pollution Liability Insurance.** Pollution liability coverage must be included when the scope of the work as defined in the Agreement includes installation, temporary storage, or disposal of any "hazardous" material that is injurious in or upon land, the atmosphere, or any watercourses; or may cause bodily injury at any time.

If required, coverage may be provided in separate policy form or by endorsement to Contractors CGL or RPL. In any form coverage must be equivalent to that provided in ISO form CG 24 15 "Limited Pollution Liability Extension Endorsement" or CG 28 31 "Pollution Exclusion Amendment" with limits of at least \$5,000,000 per occurrence and an aggregate limit of \$10,000,000.

If the scope of work as defined in this Agreement includes the disposal of any hazardous or non-hazardous materials from the job site, Contractor must furnish to Railroad evidence of pollution legal liability insurance maintained by the disposal site operator for losses arising from the insured facility accepting the materials, with coverage in minimum amounts of \$1,000,000 per loss, and an annual aggregate of \$2,000,000.

Other Requirements

- G.** All policy(ies) required above (except worker's compensation and employers liability) must include Railroad as "Additional Insured" using ISO Additional Insured Endorsements CG 20 26, and CA 20 48 (or substitute forms providing equivalent coverage). The coverage provided to Railroad as additional insured shall, to the extent provided under ISO Additional Insured Endorsement CG 20 26, and CA 20 48 provide coverage for Railroad's negligence whether sole or partial, active or passive, and shall not be limited by Contractor's liability under the indemnity provisions of this Agreement.
- H.** Punitive damages exclusion, if any, must be deleted (and the deletion indicated on the certificate of insurance), unless the law governing this Agreement prohibits all punitive damages that might arise under this Agreement.
- I.** Contractor waives all rights of recovery, and its insurers also waive all rights of subrogation of damages against Railroad and its agents, officers, directors and employees. This waiver must be stated on the certificate of insurance.
- J.** Prior to commencing the work, Contractor shall furnish Railroad with a certificate(s) of insurance, executed by a duly authorized representative of each insurer, showing compliance with the insurance requirements in this Agreement.
- K.** All insurance policies must be written by a reputable insurance company acceptable to Railroad or with a current Best's Insurance Guide Rating of A- and Class VII or better, and authorized to do business in the County of Nebraska.
- L.** The fact that insurance is obtained by Contractor or by Railroad on behalf of Contractor will not be deemed to release or diminish the liability of Contractor, including, without limitation, liability under the indemnity provisions of this Agreement. Damages recoverable by Railroad from Contractor or any third party will not be limited by the amount of the required insurance coverage.



December 14, 2010

UPRR Folder No. 2640-36

**SARPY COUNTY HIGHWAY DEPT
15100 SO 84TH ST
PAPILLION NE 68046**

Dear Sirs:

RE: Proposed construction of a new pedestrian crossing on the existing Capehart Road Overpass grade separated public road crossing structure, (DOT #433018G), at Mile Post 470.66 on the Falls City Subdivision near Fort Crook (Bellevue), in Sarpy County, Nebraska..

Please refer to the above subject matter and the documentation to cover the construction project. Attached hereto are duplicate originals of a Supplemental Agreement, for the pedestrian crossing construction project. Please return to me all of the above executed documentation for Railroad execution.

If you have any questions, please contact me.

Sincerely Yburs,

A handwritten signature in black ink, appearing to read "Paul G. Farrell", written over a circular stamp or seal.

PAUL G. FARRELL
Senior Manager Contracts
Phone: (402) 544-8620
e-mail: pgfarrell@up.com

Deb Houghtaling

Fred Uhe
Chief Deputy

Sarpy County Clerk

Renee Lansman
Assistant Chief Deputy

1210 Golden Gate Drive • Papillion, Nebraska 68046-2895
Phone: 402-593-2105 • Fax: 402-593-4471 • Website www.Sarpy.com • Email: Clerk@sarpy.com

February 2, 2011

Union Pacific Railroad Company
Paul G. Farrell, Sr. Mngr. Contracts
1400 Dodge Street, MS 1690
Omaha NE 68179-1690

RE: Sarpy County Supplemental Agreement for Use of Right-of-Way

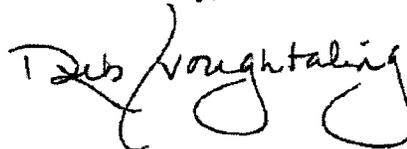
Resolution 2011-042: Authorize Chairman to sign supplemental agreement for the use of Union Pacific right-of-way near Capehart Road and Fort Crook Road. Dennis Wilson, Engineer

MOTION: Resolved by Warren, seconded by Thompson, to approve the resolution for the supplemental agreement to construct a new pedestrian crossing as part of the Offutt Bicycle Route Connector on the existing Capehart Road Overpass. Ayes: Hike, Thompson, Nekuda & Warren. Nays: None. Absent: Richards.

Please find enclosed two (2) originals of the subject agreement which have been approved and signed by the Chairman of the Board. Upon completion please provide **one original** for Sarpy County records.

Mail to: Sarpy County Clerk
Attn: Chris Vance
1210 Golden Gate Dr.
Papillion NE 68046-2895

Sincerely,



Deb Houghtaling
Sarpy County Clerk

Enclosures (2)
DH/kk



OFFUTT BICYCLE ROUTE CONNECTOR

PROJECT NO.
137624

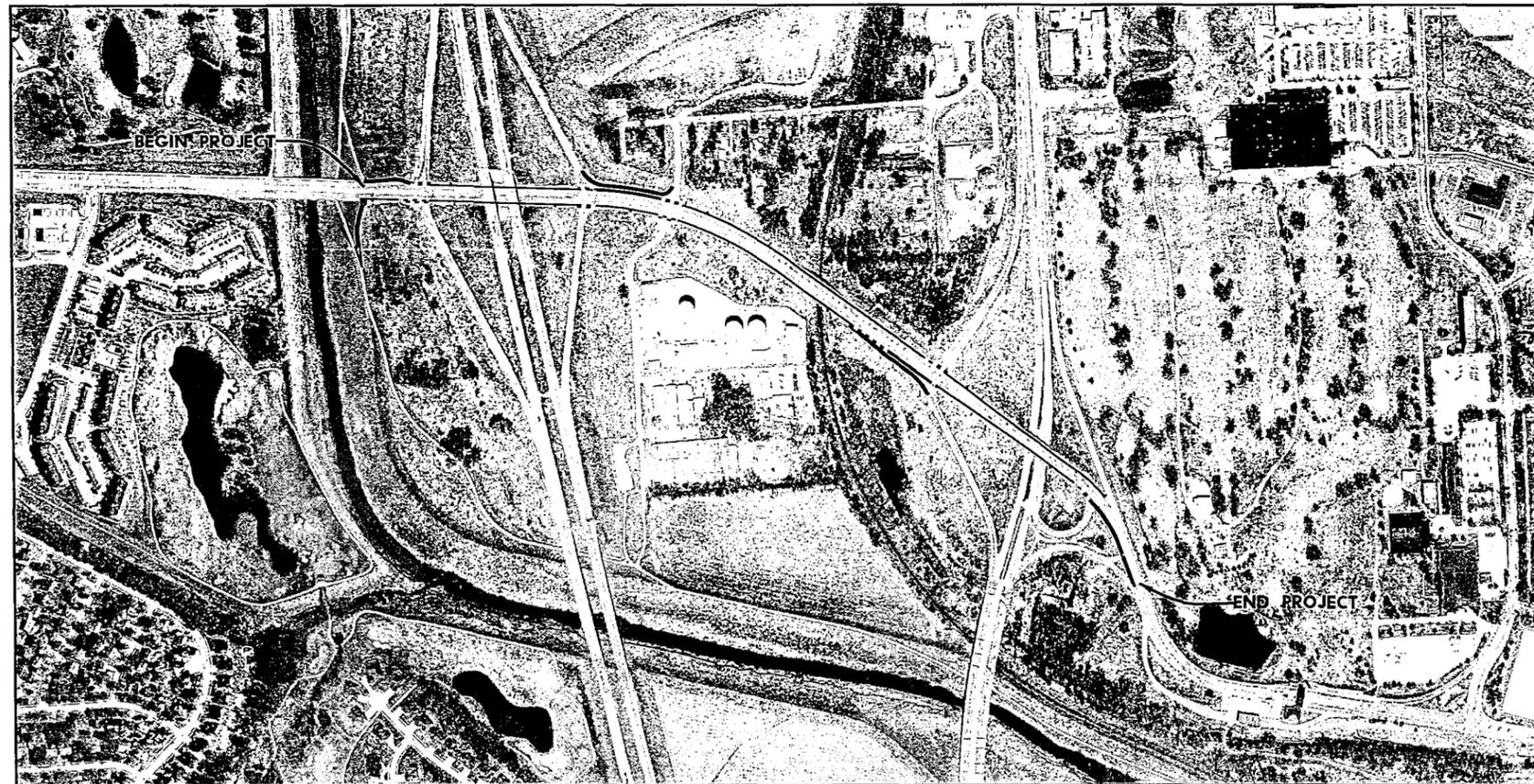
BELLEVUE, NEBRASKA
AUGUST 30, 2010

SHEET INDEX

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- 2T TYPICAL SECTIONS
- 2H ALIGNMENT & CONTROL
- 2L1 - 2L4 CONSTRUCTION / GEOMETRICS / SIGNING
- 3-7 SPECIAL PLANS
- XI - X8 CROSS SECTIONS

NDOR STANDARD PLANS

- PLAN 301-R10 PAVEMENT DETAILS
- PLAN 502 SILT FENCE DETAILS
- PLAN 920-R5 TRAFFIC CONTROL, CONSTRUCTION AND MAINTENANCE
- PLAN 924 TYPICAL URBAN TRAFFIC CONTROL PLAN



LOCATION MAP

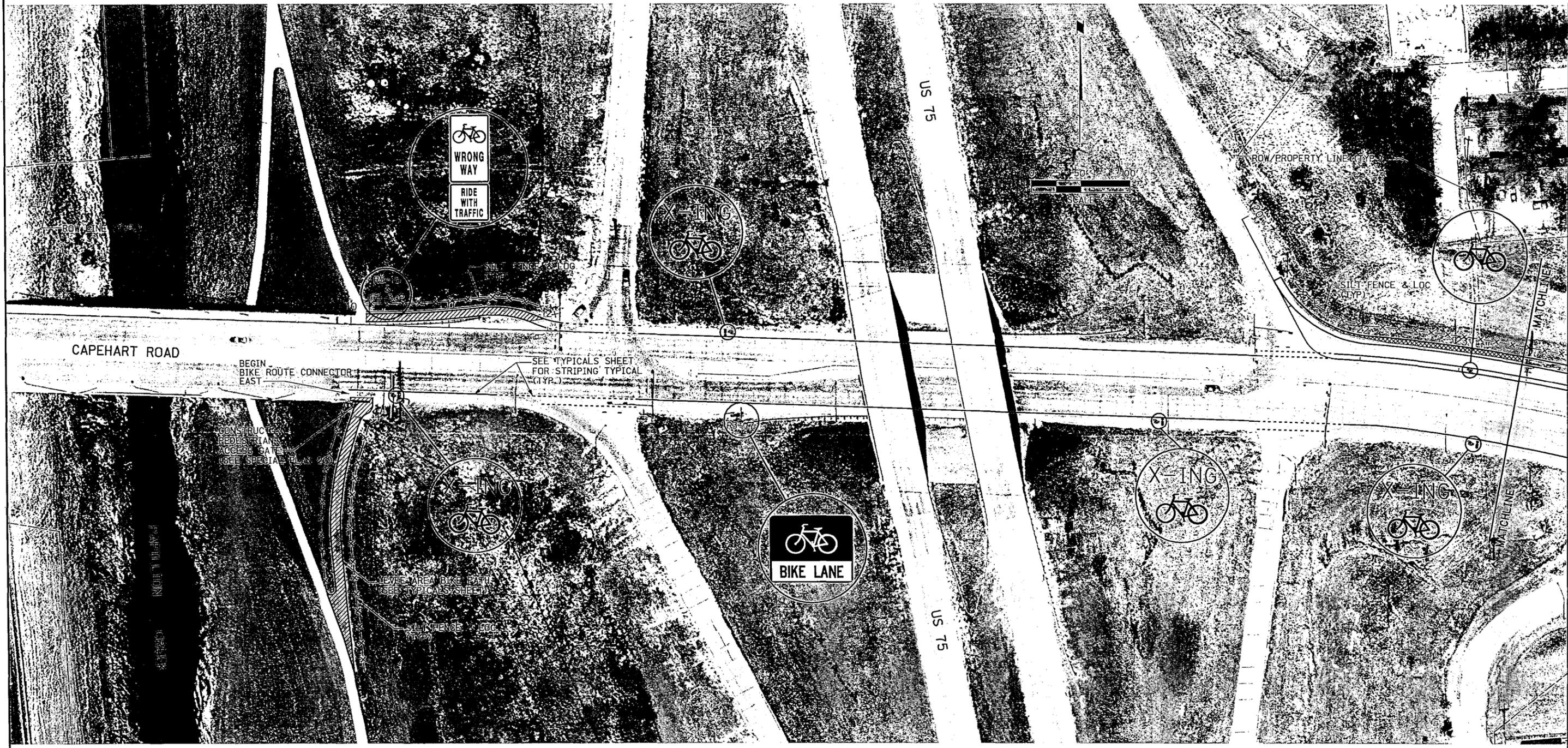
ROADWAY	2006 ADT
CAPEHART RD. - HWY 75 to FORT CROOK RD.	15,500
CAPEHART RD. - EAST of FORT CROOK RD.	13,500



REMOVE GATE		
STATION	TO	STATION
		EACH
		1

Note: Contractor shall remove and salvage the existing gate for Papio NRD.

BUILD FABRIC SILT FENCE-LOW POROSITY, TYPE "ST", PLAN 502				
STATION	TO	STATION	SIDE	DESCRIPTION
			LT & RT	ALONG LEVEE AREA TRAIL
10+15		12+14	LT	-
20+70		23+62	LT	-
				LIN. FT.
				470
				198
				261



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BUILD CONCRETE BIKEWAY (SEE SHEET 2-T)					
STATION	TO	STATION	SIDE	WIDTH	SQ. YDS.
	*		RT	10'	319
10+15		11+97	LT	8'	156

BUILD 9" CONCRETE PAVEMENT				
STATION	TO	STATION	SIDE	SQ. YDS.
20+70		23+62	LT	343

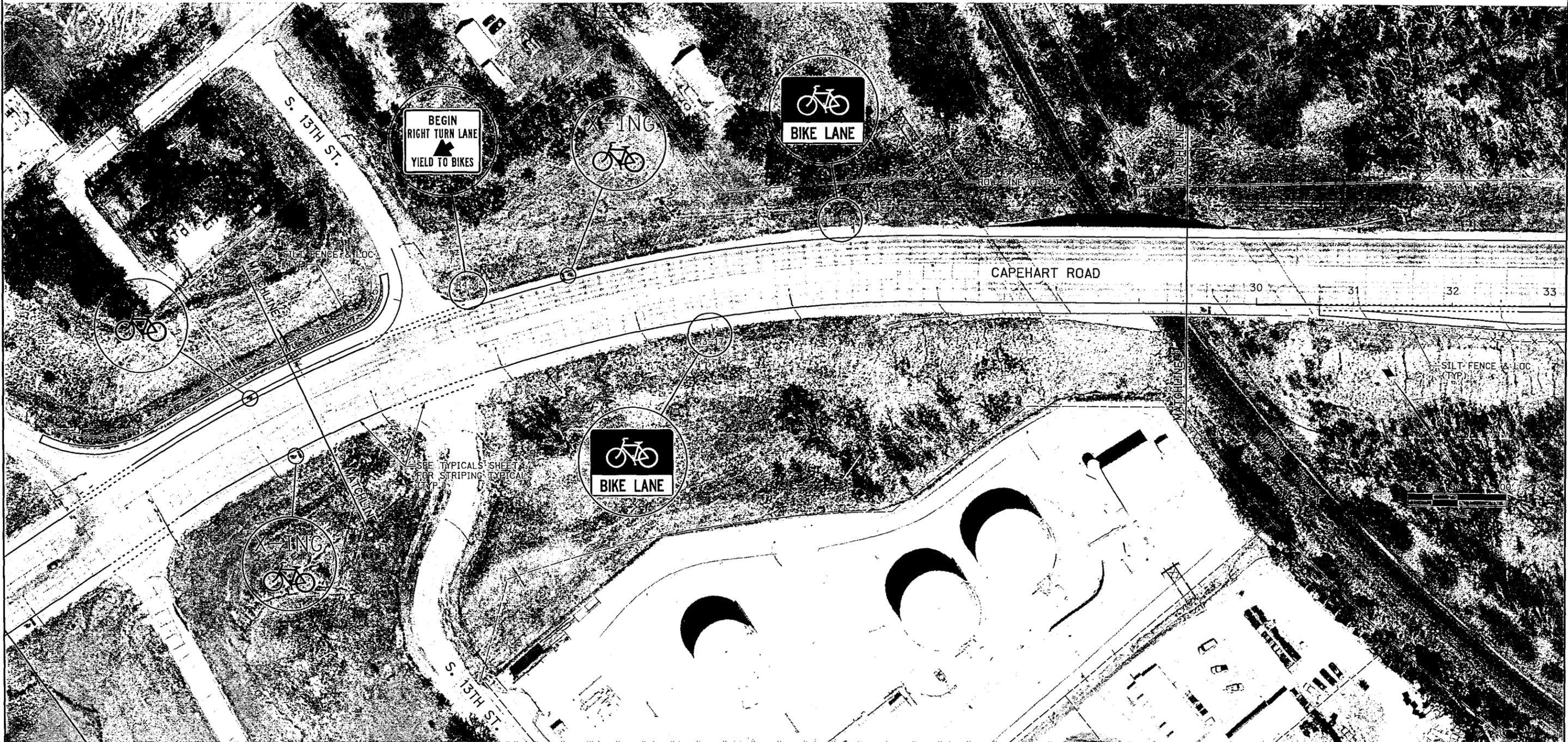
BUILD PEDESTRIAN ACCESS GATE, SPECIAL PLAN 2C		
STATION	TO	STATION
		EACH
		1

BUILD BIKEWAY HANDRAIL, SPECIAL PLAN 1C			
STATION	TO	STATION	LIN. FT.
10+15		11+33	118
			HEIGHT
			4.5'



CONSTRUCTION

REMOVE FENCE			
STATION	TO	STATION	LIN. FT.
RAILROAD BRIDGE RAILING FENCE		NORTH	69.5
RAILROAD BRIDGE RAILING FENCE		SOUTH	69.5



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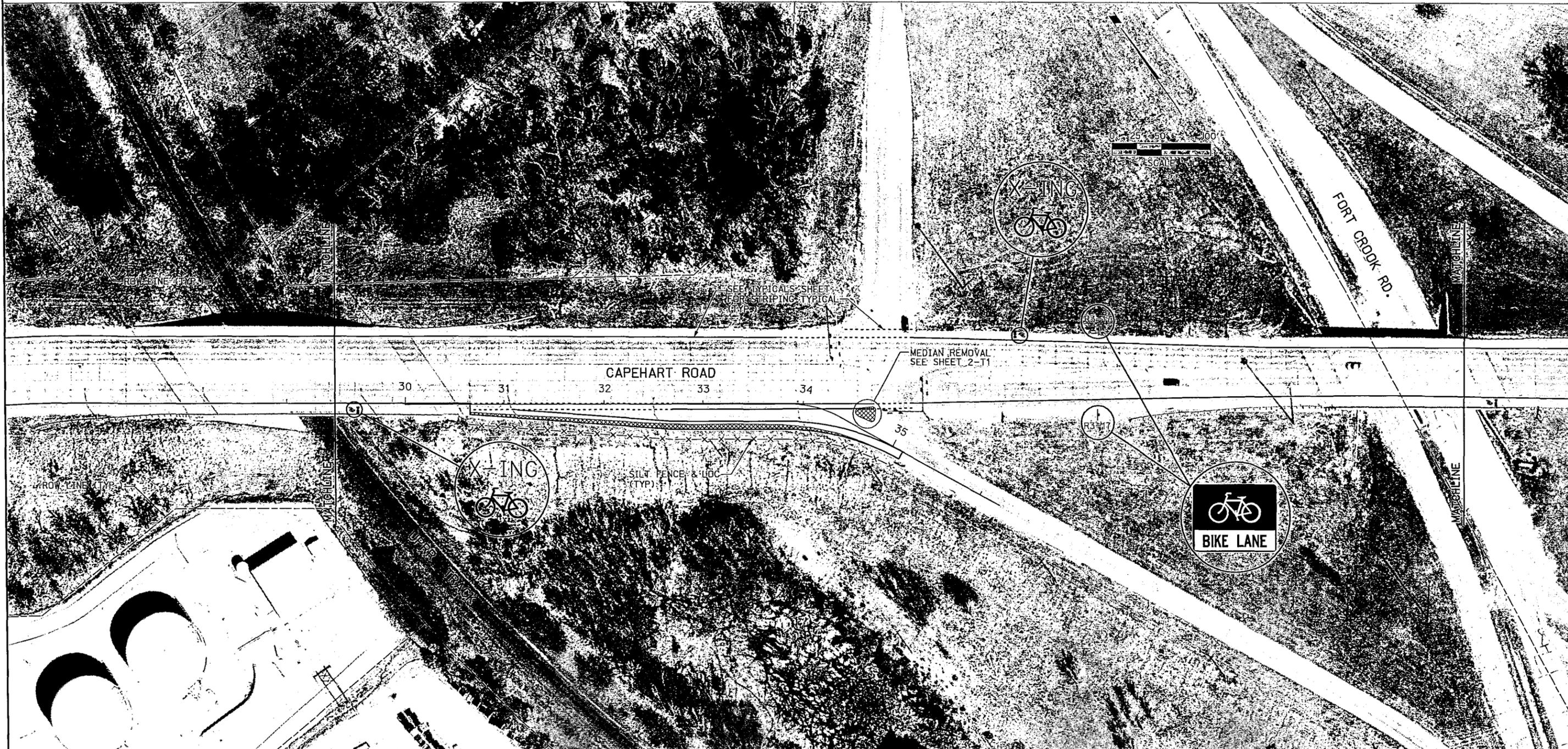
BUILD CHAIN LINK FENCE, SPECIAL PLAN 4C		
LOCATION	LIN. FT.	HEIGHT
LT & RT BRIDGE RAILING OVER UPRR TRACKS	320	5'

BUILD 9" CONCRETE PAVEMENT				
STATION	TO	STATION	SIDE	SQ. YDS.
23+62		25+32	LT	66

BUILD FABRIC SILT FENCE-LOW POROSITY, PLAN 502					
STATION	TO	STATION	SIDE	DESCRIPTION	LIN. FT.
23+62		25+32	LT	-	124



CONSTRUCTION



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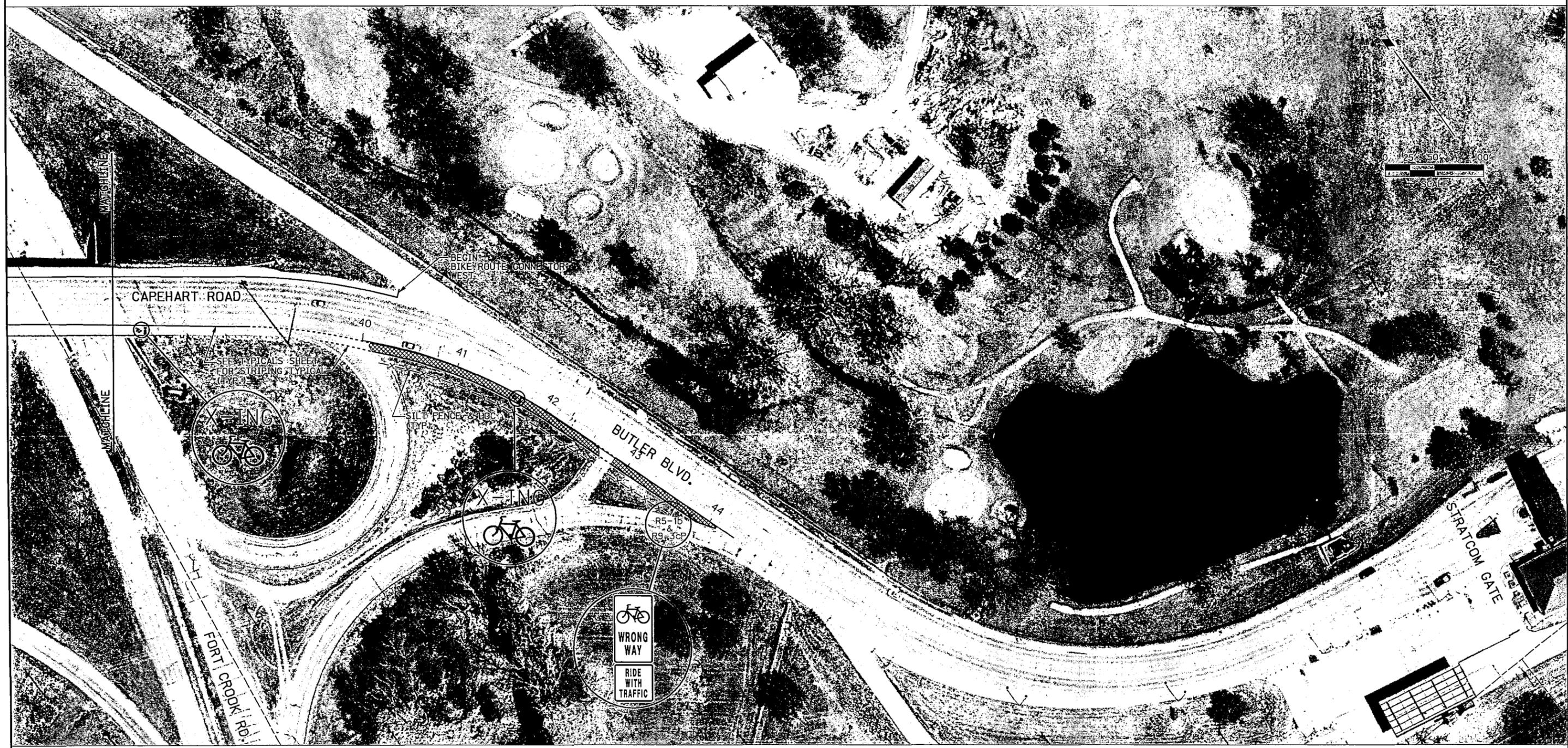
BUILD CHAIN LINK FENCE, SPECIAL PLAN 4C		
LOCATION	LIN. FT.	HEIGHT
LT & RT BRIDGE RAILING OVER FORT CROOK ROAD	290	5'

BUILD 9" CONCRETE PAVEMENT			
STATION TO	STATION	SIDE	SQ. YDS.
30+66	35+12	RT	165
MEDIAN REMOVAL AREA		-	24

BUILD FABRIC SILT FENCE-LOW POROSITY, PLAN 502				
STATION TO	STATION	SIDE	DESCRIPTION	LIN. FT.
30+66	35+12	RT	-	432



CONSTRUCTION



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BUILD 9" CONCRETE PAVEMENT				
STATION	TO	STATION	SIDE	SQ. YDS.
40+10		42+70	RT	221
42+85		44+08	RT	104

-x- BUILD FABRIC SILT FENCE-LOW POROSITY, PLAN 502						
STATION	TO	STATION	SIDE	DESCRIPTION	LIN. FT.	
40+22		42+61	RT	-	232	
42+87		43+58	RT	-	71	



CONSTRUCTION